

# 1700 N Front Street

Civic Design Review Presentation | September 14, 2021



**concrete** |



*the*  
**FISHTOWN**  
*collective*

**URBY** | **NORR**

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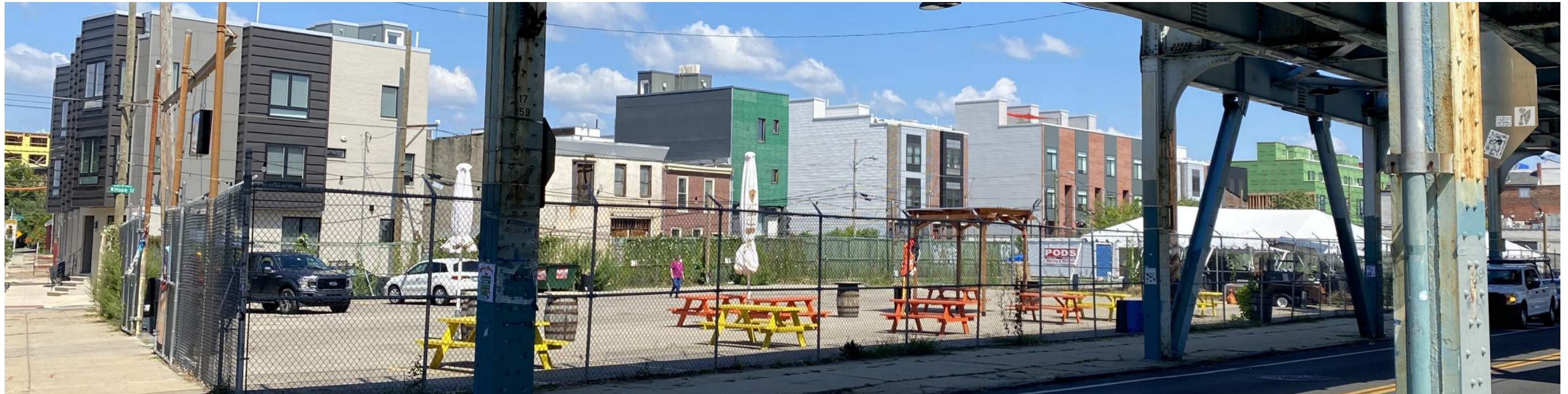
Site - Aerial



# Site - Aerial Oblique



# Site Photos - N. Front St.



# Site Photos - Cecil B Moore Ave.



# Site Photos - Palmer St.



# Site Photos - Hope St.





# Design Statement

The project site is bounded by North Front Street to the east, Palmer Street to the north, Hope Street to the west, and Cecil B. Moore Avenue to the south. The 1700 North Front Street site will be developed as a five-story residential rental building with street-level retail running along Cecil B. Moore Avenue and North Front Street. In addition to retail, the ground floor will include residential amenities, lobby, and back-of-house functions, and several residential units along the southern part of Hope Street.

The starting point for the mass is a brick volume extruded to all property lines. At strategic locations, slices through the brick create setbacks and reveal contrasting colors and materials to break up the elevations along the property's perimeter. The brick and continuous window pattern speak to the area's history, still apparent at structures like the tower at the corner of Cecil B. Moore Avenue and Hope Street and the low building nearby at West Columbia Avenue and North Front Street.

The landscaped court along North Front Street creates not just a building entry but a sizeable outdoor space designed to accommodate outdoor seating for adjacent retail spaces. Multiple trees in the court form a welcome buffer from the busy street and train line above while also yielding a pleasant aesthetic for train commuters. Masonry and synthetic stucco finished with a green color blend in with the vegetation.

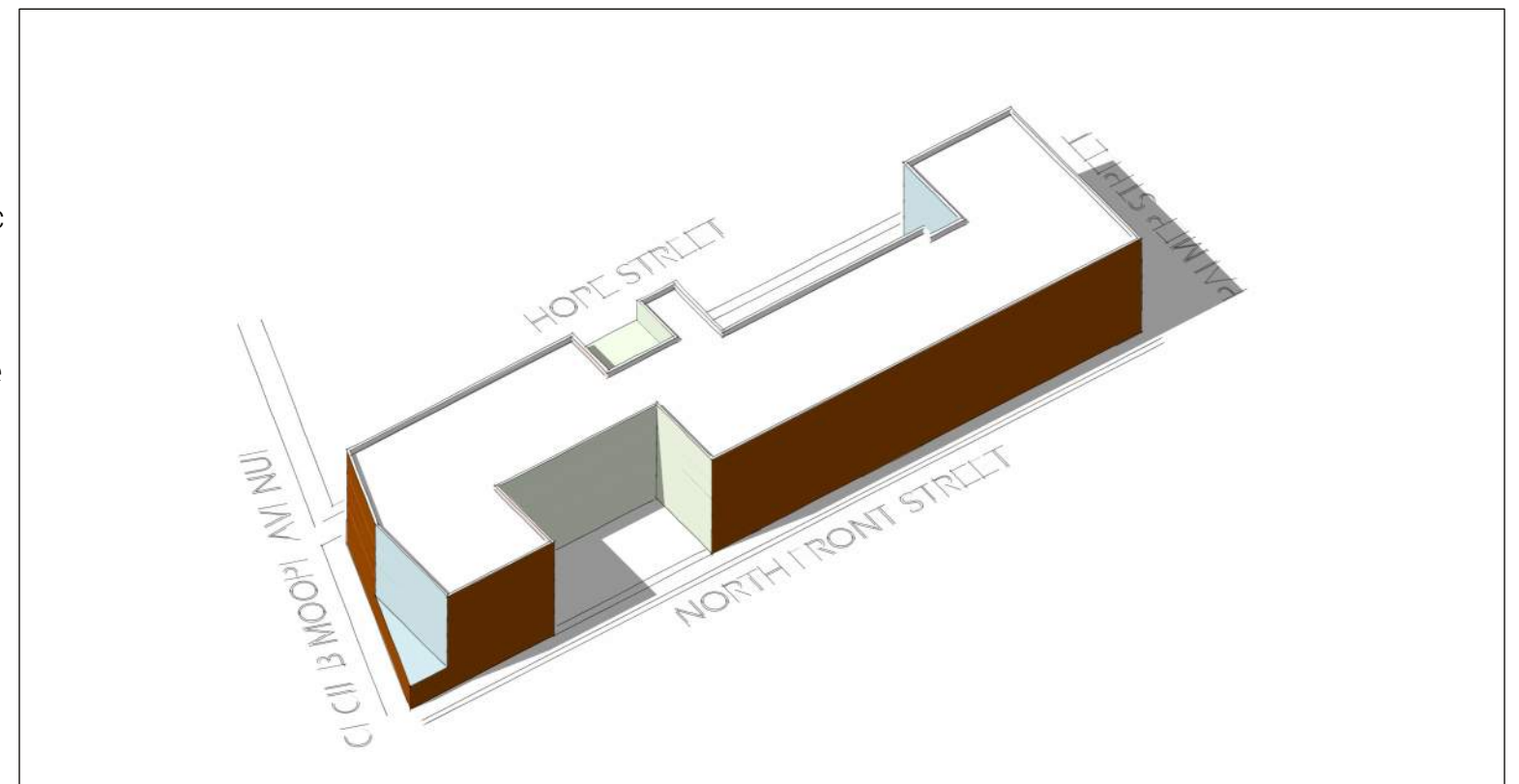
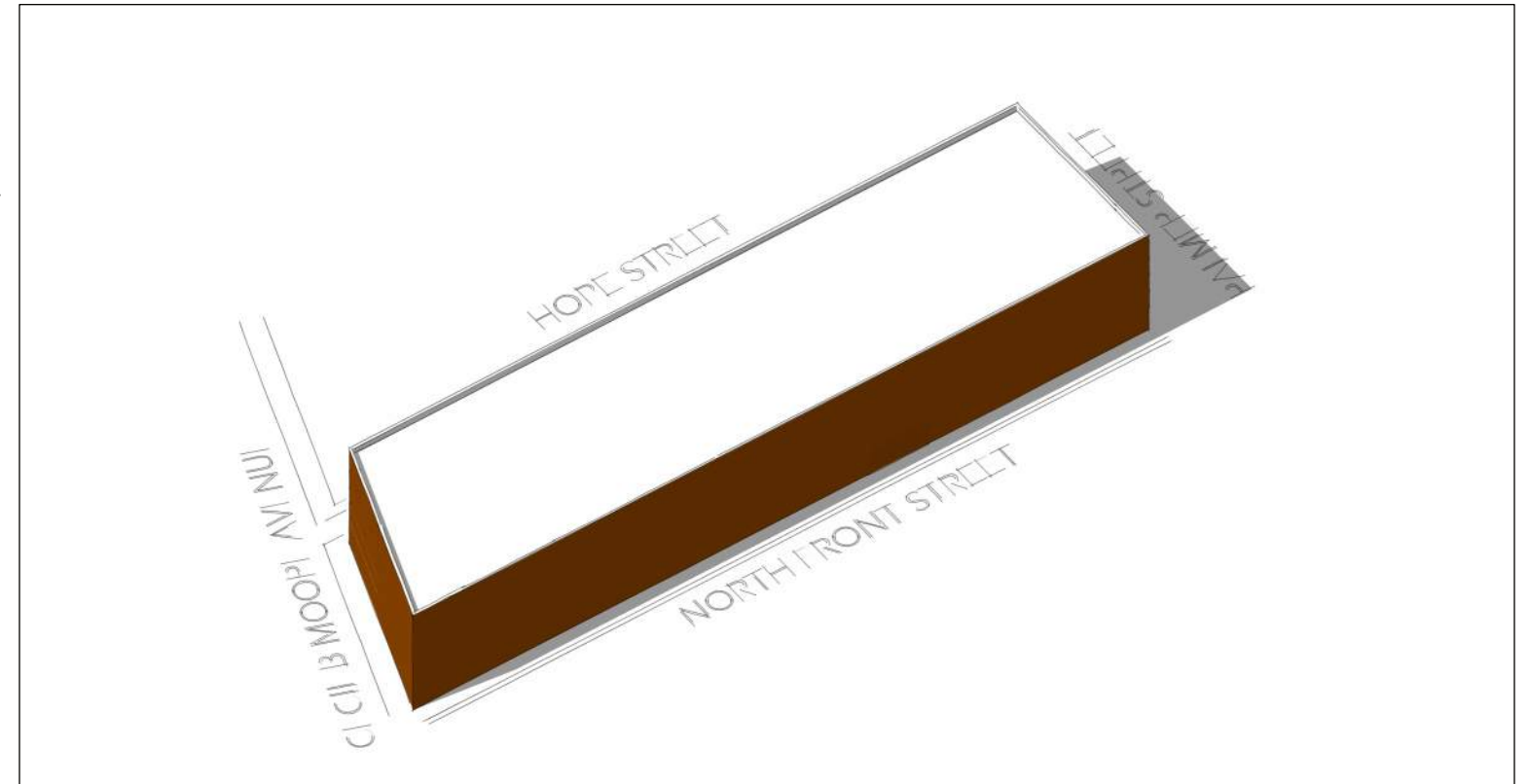
Along similar lines, the court on Hope Street creates a large pocket of light and air along an otherwise very narrow street. The blue shade of the masonry and synthetic stucco facade materials refer to the color of the steel structure of the elevated train.

At the prominent corner of Cecil B. Moore Avenue and North Front Street, retail extends to the property line while the floors above step back. This squaring-off of the upper floors opens up the view of the Cecil B. Moore "square" from the elevated train while the ground-floor retail completes the block along the right-of-way line.

The continuous window rhythm at the proposed building relates to the punched-window aesthetic common in the historic brick structures throughout the area. A slightly higher windowsill along the North Front Street elevation provides some protection against the considerable noise of the adjacent train.

A steel channel detail at the top of the first-floor elevation creates a clear dividing line between the facades of the ground and upper floors much like the steel relieving angles and lintels common in many industrial buildings. This datum also provides an opportunity to cleanly break the ground-floor facade finishes from the strong brick materials above.

Additional details in the brick help to add interest along large elevations. A continuous soldier course runs above the first-floor windows, and rectangular recesses in the brick complete the window pattern where glass is not feasible due to apartment layouts. Alternating angled bricks near the top of the building create a clear end of the block.



# Project Summary



CORNER OF FRONT ST + CECIL B. MOORE AVE

1700 N FRONT STREET IS A NEW MIXED USE DEVELOPMENT ZONED CMX-2.5. THE PROPOSED PROJECT HAS GROUND FLOOR COMMERCIAL, RESIDENTIAL UNITS, AND GREEN SPACE.

- **RESIDENTIAL**
  - 204 TOTAL UNITS
  - 28 STUDIOS
  - 141 1-BEDROOMS
  - 35 2-BEDROOMS
- **COMMERCIAL SPACE**
  - 16,429 SF
- **BUILDING GROSS AREA**
  - 172,844 SF

# Zoning Analysis

AREA REGULATIONS			
<b>LOT INFORMATION</b>			
ADDRESS: 1700-44 N FRONT ST, PHILADELPHIA, PENNSYLVANIA, 19122 1700-38 N Front OPA # 885843440 & 1740-44 N Front OPA #885799760 TOTAL LOT AREA: 45,273.9 SF or 1.03932 ACRES (P.D.S.). 45,463.7 SF or 1.04370 ACRES (U.S.S.) U.S.S. TO P.D.S UNITS CONVERSION FACTOR IS 45,273.9 SF P.D.S. SITE / 45,463.7 SF U.S.S. SITE = 0.9958252 ZONING: CMX-2.5, COMMERCIAL MIXED USE; COUNCIL DISTRICT 7, WARD 18			
<b>USE RESTRICTIONS</b>			
		FIRST FLOOR: VACANT COMMERCIAL (ALLOWED) & RESIDENTIAL LOBBY/RESIDENTIAL AMENITY (ALLOWED - IF GREATER THAN 30' FROM FRONT LOT LINE, MEASURED FROM CECIL B MOORE AVENUE)	
		SECOND THROUGH FIFTH FLOORS: MULTI-FAMILY RESIDENTIAL (ALLOWED)	
		ROOF: UNOCCUPIED	
<b>CHAPTER 14-701 DIMENSIONAL STANDARDS</b>			
	ZONING CODE SECTION	REQUIRED/ ALLOWED	PROPOSED
MAXIMUM OCCUPIED AREA (% OF LOT)	Table 14-701-3	80% corner lot	80%
MINIMUM FRONT YARD DEPTH (FT)	Table 14-701-3	Must be built to front lot line; 0'-0"; Cecil B Moore Ave is front lot (Note 1 & 2)	Built to front lot line 0'-0"
MINIMUM SIDE YARD WIDTH, EACH (FT)	Table 14-701-3	5'-0" if used is not applicable (Note 1)	0'-0" or 5'-0" or greater where used
MIN. REAR YARD DEPTH (FT)	14-701(1)(d)(.1)(.b)	The greater of 9 ft. or 10% of lot depth is not applicable; 0'-0" or more allowed (Note 1)	0'-0" on W Palmer St
MAXIMUM HEIGHT (FT)	14-603(7)(.d)	55'-0" base, up to 62'-0" with mixed income bonus (Note 3)	62'-0"
MAXIMUM UNITS ALLOWED	14-602(4)(a)[8](.b)	167 base increased to 208 (Note 4)	204 units

<b>CHAPTER 14-705 LANDSCAPE AND TREES</b>			
STREET TREE REQUIREMENTS	14-705(2)	5 (Note 5)	4 on Cecil B Moore Ave & 1 on Palmer St
<b>CHAPTER 14-802 MOTOR VEHICLE PARKING RATIOS</b>			
TOTAL CAR PARKING	Table 14-802-2	0 (Note 6)	0
<b>CHAPTER 14-804 BICYCLE PARKING RATIOS AND STANDARDS</b>			
BICYCLE PARKING	Table 14-804-1	1 bike for 3 units: 62 bikes for 204 units + 2 bikes for other commercial uses = 64 (Note 7)	70 bikes
<b>CHAPTER 14-806 OFF STREET LOADING</b>			
OFF STREET LOADING SPACES	Table 14-806-1 & 14-806-3	2 (Note 8)	2
<b>CHAPTER 14-904 SIGNAGE</b>			
ALLOWED WALL SIGNAGE SQ. FT.	Table 14-904-1	872 SF on N Front St (only long side); 1,188 SF on Hope St; 345 SF on Cecil B Moore Ave frontage; 324 SF on W Palmer St frontage. 1 projecting signed allowed per frontage (Note 9)	See elevations and Signage Schedule
<b>CHAPTER 14-703 FORM AND DESIGN</b>			
SITE DESIGN	14-703(2)(c) & 14-703(5)(a)	Each principal building shall have one or more entry doors facing and visible from an adjacent public street. (Note 11)	The building has one or more entries visible from an adjacent public street.
BUILDING DESIGN	14-703(2)(c) & 14-703(5)(b)	Between sidewalk and 25' of height at least 30% of the wall area must meet Façade Articulation (Note 11)	Façade Articulations exceeds 30%. See elevations.

# Zoning Analysis

AREA REGULATION NOTES
<p><b>Note 1:</b> Per § 14-701(1)(d)(.3) "When a property is completely surrounded by streets, only the front yard requirements of the zoning district shall apply." PCPC confirmed with Zoning Attorney they selected Cecil B. Moore as the front yard.</p>
<p><b>Note 2:</b> Per § 14-602(4)(a)[3] Notes of Table 14-606-2: "In the CMX-2 and CMX-2.5 districts, in order to promote active uses at the street level, buildings must contain a use other than residential and other than parking along 100% of the ground floor frontage and within the first 30 ft. of building depth, measured from the front building line. If the property is bounded by two or more streets, only the primary frontages as designated in § 14-701(1)(d)(.4) (Primary Frontage) shall be subject to this requirement." Primary Frontage is Cecil B. Moore.</p>
<p><b>Note 3:</b> Per § Table 14-701-3 the maximum building height for CMX-2.5 is 55'-0". Per § 14-702(7)(c) Mixed Income Housing Bonus Tables. "(.1)... A property may take advantage of a height bonus, a gross floor area bonus, and a dwelling unit density bonus, either alone or in combination, if so provided by the tables in subsection (.3), below. A property may not take advantage of any one of these bonuses multiple times and may not earn bonuses under both the low-income and moderate-income standards." The second Table allows a 7' increase in height permitted for RM-1, CMX-1, CMX-2, CMX-2.5. If this Table's height is added to the total: <b>55' base + 7' mixed income bonus = 62'</b></p>
<p><b>Note 4:</b> Per § 14-602(4)(a)[8](.b) A minimum of 270 sq. ft. of lot area is required per dwelling unit for buildings greater than 45 ft. in height. Per § 14-702(7)(c) Mixed Income Housing Bonus Tables. "(.1)... A property may take advantage of a height bonus, a gross floor area bonus, and a dwelling unit density bonus, either alone or in combination, if so provided by the tables in subsection (.3), below. A property may not take advantage of any one of these bonuses multiple times and may not earn bonuses under both the low-income and moderate-income standards." The last Table allows a 25% increase in units permitted for Moderate Income Units. Payment-in-lieu per § 14-702(7)(b)(.6)(.c)(i) or (ii): Moderate Income pay greater of \$20 x 45,273.9 SF lot = \$905,478 or additional units earned by this bonus = 41 units x \$25,000 per unit = \$1,025,000. <b>45,273 SF P.D.S. lot / 270 = 167 units allowed x 25% increased per § 14-702(7)(c)(.3) Density Table [+41 units] = 208 units allowed (with Moderate Income Bonus).</b></p>
<p><b>Note 5:</b> Per § 14-705(2)(c)(.2)(.a) At least one street tree per 35 feet of linear frontage shall be provided. Street trees may be placed at regular or irregular intervals, provided that there is at least 15 ft. of space between tree trunks. The linear frontage on Cecil B Moore Avnue is 115' P.D.S. / 35' = 4 street trees required. On W Palmer Street 108' P.D.S. / 35' = 4 street tree required, however, due to Philadelphia Streets Department requirements a reduction of 3 street trees is required therefore 1 street tree is provided on Palmer Street. N Front Street is under Market-Frankford Elevated Train no street trees allowed. No trees proposed on Hope Street.</p>
<p><b>Note 6:</b> Table 14-802-2 The minimum required parking spaces for Multi-Family housing in the CMX-2.5 district is 0; for Office Uses in CMX-2.5 is 0; for Retail Sales Uses in CMX-2.5 is 0. Per § 14-803(5)(d)(.2) "Where a five ft. wide perimeter landscape area is not feasible based on existing site or topography constraints, as an alternative to the tree and shrub plantings required by § 14-803(5)(d)(.1) above, a decorative masonry wall or ornamental fence at least three ft. in height may be installed. The decorative wall or ornamental fence may be installed on a berm if the combined height of berm and wall or fence is at least three ft. in height. (.a) The decorative masonry wall must be constructed of brick, stone, decorative block wall, or concrete block with stucco finish, but shall not be constructed of uncolored or painted concrete block." <b>A 3' - 4' tall brick screen wall along property line at Hope Street is provided for the drop-off.</b></p>

<p><b>Note 7:</b> Per § 14-804(1) One bicycle parking space is required per every three dwelling units. And one bicycle for every 10,000 SF or fraction thereof of other uses (office/retail/mercantile). Commercial square footage is between 7,5001 - 20,000 SF requiring 2 bikes per Table 14-804-1. <b>204 units x 0.3 = 62 bicycles + 2 commercial = 64 total bicycle parking spaces required.</b></p>
<p><b>Note 8:</b> Per Table 14-806-1 The minimum number of off street loading spaces required for commercial districts, for buildings with use office, visitor accommodation, or residential, and with a gross floor area less than 100,000 SF shall be 0, and for buildings with 100,000 to 150,000 SF shall be 1, and for buildings with 150,001 to 400,000 SF shall be 2. Loading Space shall be 11' wide x 60' long x 14' high per Table 14-806-3. <b>2 loading berth are provided.</b></p>
<p><b>Note 9:</b> Accessory Sign Controls per Table 14-904-1 in CMX-2 and CMX-2.5 lots with more than one building frontage for Wall, Projecting and Marquee Signage the short sides allowed 3 SF of signage per linear foot of building frontage and long side allowed 2 SF of signage per linear foot of building frontage; maximum height of signage the lower of the roof line or second floor window sill. 100 SF total allowed for Freestanding sign with max height 20'. 1 project sign allowed per frontage. Static Illumination allowed. Animated Illumination not allowed. Mechanical Motion is allowed. <b>The linear frontage on N Front Street is 436' P.D.S. = 436 x 2 SF = 872 SF of wall signage allowed on N Front Street frontage. The linear frontage on Cecil B Moore Avenue is 115' P.D.S. = 115 x 3 SF = 345 SF of wall signage allowed on Cecil B Moore Avenue frontage. The linear frontage on W Palmer Street is 108' P.D.S. = 108 x 3 SF = 324 SF of wall signage allowed on W Palmer Street frontage. The linear frontage on Hope Street is 396' = 396 x 3 SF = 1,188 SF of wall signage allowed on Hope Street frontage. 1 projecting signed allowed per frontage.</b></p>
<p><b>Note 10:</b> Per § 14-500 1700-44 N Front Street is not located in an Overlay District. It is just outside the NCA, North Delaware Avenue Overlay District Per § 14-503(8)(a).</p>
<p><b>Note 11:</b> Per § 14-703(2)(c) both (.1) &amp; (.2) are applicable for Form and Design. § 14-703(3) Certification. "Zoning permit applications must, if subject to the provisions of this § 14-703 (Form and Design), include a site plan and elevation drawings prepared by a licensed architect or a licensed professional civil engineer that demonstrates compliance with the standards of this § 14-703 (Form and Design)." The requirement of 14-703(5)(a) are meet, relevant section states: "(a) Site Design. (.1) Entries. Each principal building shall have one or more entry doors facing and visible from an adjacent public street." <b>The proposed building has one or more entry doors visible from an adjacent public street.</b> The requirements of § 14-703(5)(b)(.1) are met, which states "Between the average ground level and a height of 25 ft., a minimum of thirty percent (30%) of the wall area must include one or more of the following features listed in subsections (.a) through (.c): (.a) Ornamental and structural detail; (.b) Projections, recesses, bays, overhangs, or other variations in planes; or (.c) Transparent windows or other transparent glazed area." <b>The Facade Articulation includes transparent windows and variations in planes.</b></p>

# Notice of CDR Referral

## The CDR PROJECT APPLICATION FORM

Note: For a project application to be considered for a Civic Design Review agenda, complete and accurate submittals must be received no later than 4 P.M. on the submission date. A submission does not guarantee placement on the agenda of the next CDR meeting date.

L&I APPLICATION NUMBER: **ZP-2021-006063**

What is the trigger causing the project to require CDR Review? Explain briefly.

Per 14-304(5)(b)(.1) the applicant's property meets the criteria in Table 14-304-2. The project meets case 1: located in any district, creates more than 100,000 SF and 100 dwelling units.

### PROJECT LOCATION

Planning District: <u>Lower North</u>	Council District: <u>7</u>
Address: <u>1700-42 N Front Street</u>	
<u>Philadelphia, PA 19122</u>	
Is this parcel within an Opportunity Zone?	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Uncertain
If yes, is the project using Opportunity Zone Funding?	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>

### CONTACT INFORMATION

Applicant Name: <u>Adam Laver</u>	Primary Phone: <u>(215) 569-5764</u>
Email: <u>laver@blankrome.com</u>	Address: <u>One Logan Square, 130 North 18<sup>th</sup> St.</u>
<u>Philadelphia, PA 19103</u>	
Property Owner: <u>1714 Dealers Associates GP LLC</u>	Developer: <u>1700 North Front LLC</u>
Architect: <u>NORR</u>	

### SITE CONDITIONS

Site Area: <u>45,463 SF U.S.S.; 45,273 SF P.D.S.</u>
Existing Zoning: <u>CMX-2.5</u> Are Zoning Variances required? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
Proposed Use: <i>Area of Proposed Uses, Broken Out by Program (Include Square Footage and # of Units):</i> 204 dwelling units (multi-family) : 123,218 SF apartments; 5,364 SF amenities (excludes ground floor outdoor space. Circulation: 27,833 SF. Commercial / Retail space: 16,429 SF. Gross Building Area: 172, 844 GSF.  <i>Proposed # of Parking Units:</i> 70 Class 1A bicycle spaces. 0 automobile spaces.

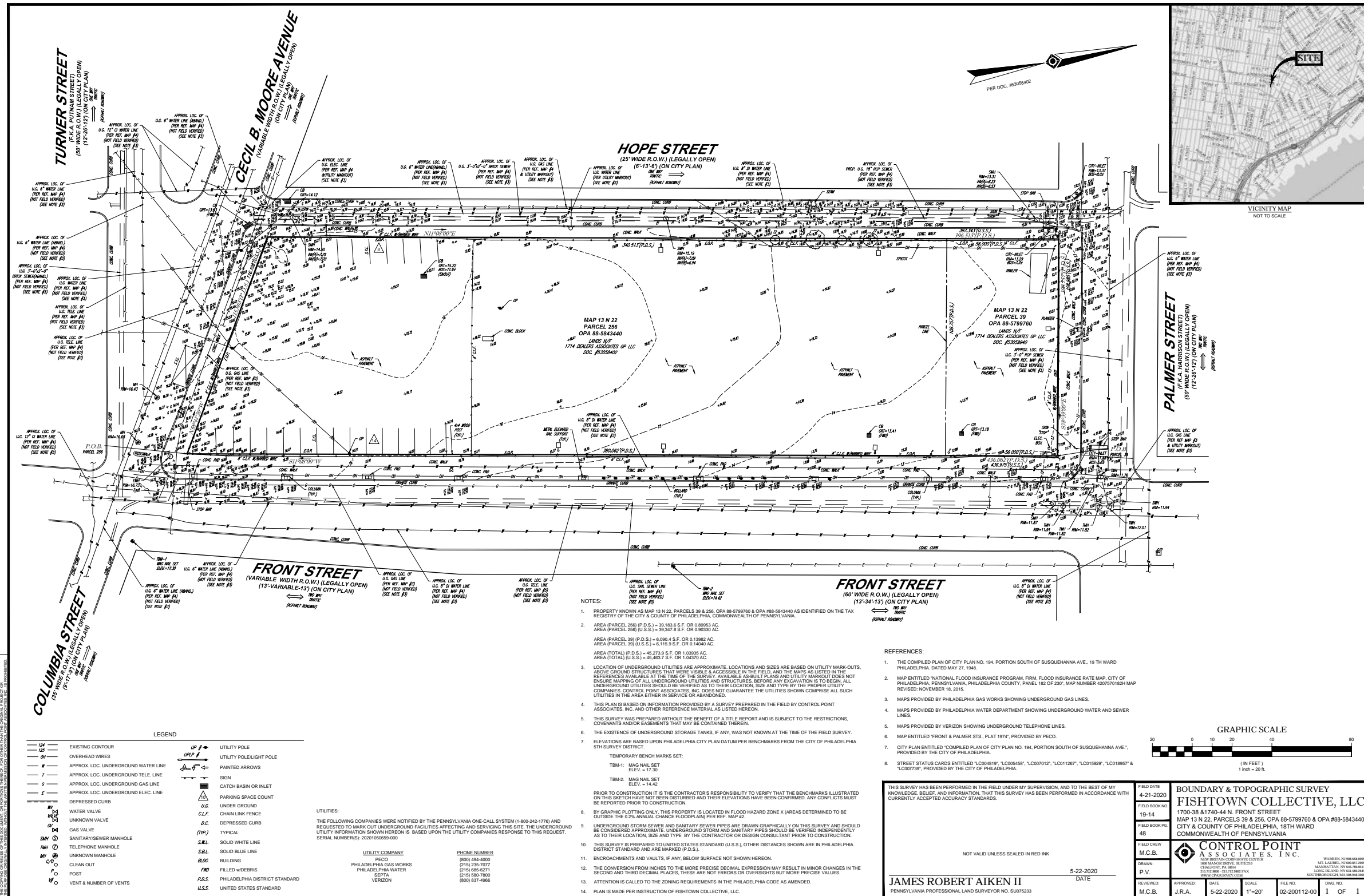
### COMMUNITY MEETING

Community meeting held: Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
If yes, please provide written documentation as proof.
If no, indicate the date and time the community meeting will be held: Date: <u>Date TBD</u> Time: _____

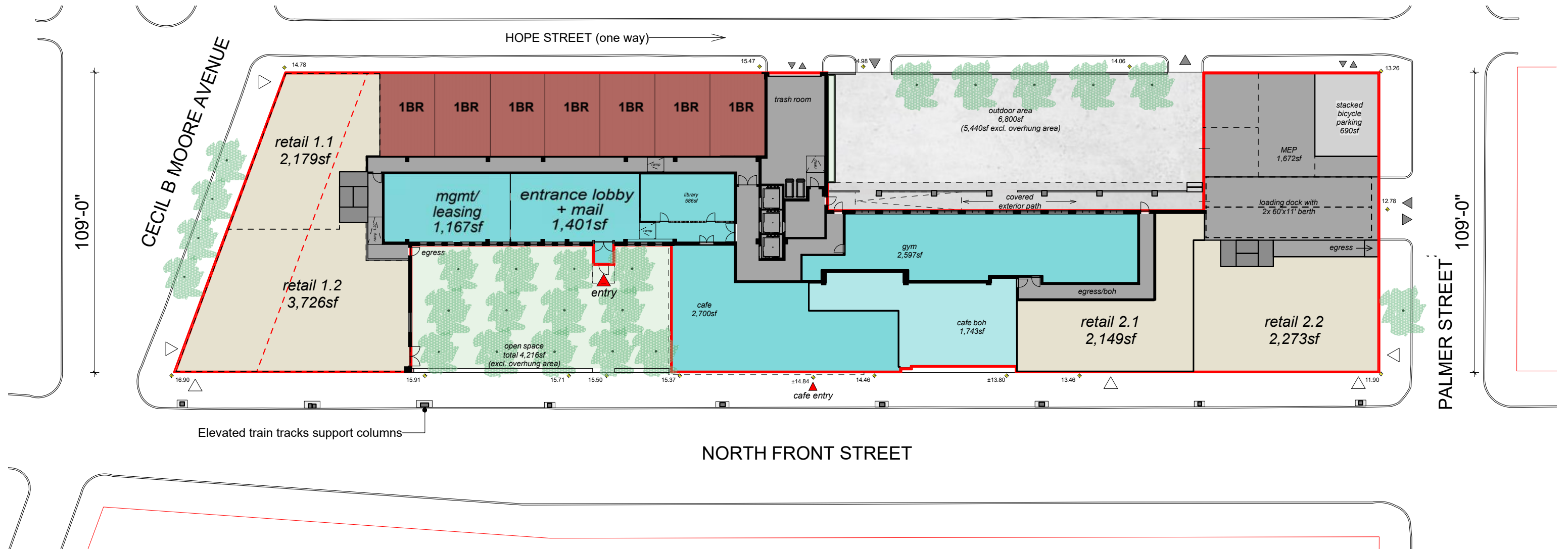
### ZONING BOARD OF ADJUSTMENT HEARING

ZBA hearing scheduled: Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> NA <input type="checkbox"/>
If yes, indicate the date hearing will be held: Date: _____

# Civil Site Plan - Existing



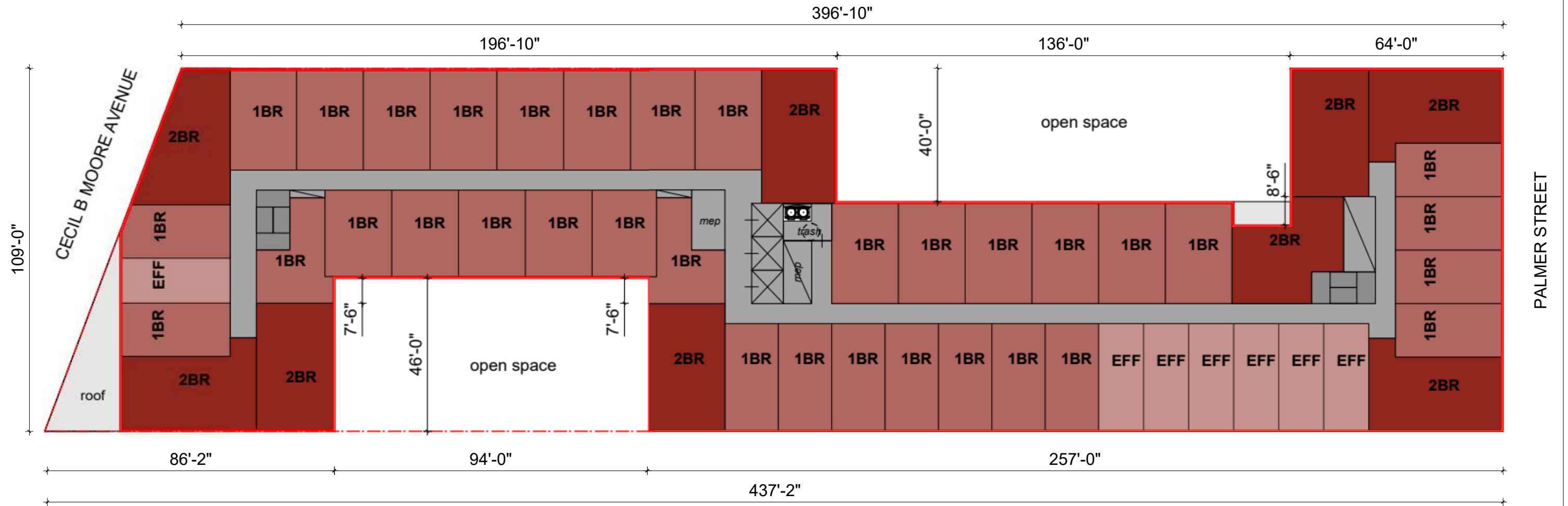
# Ground Floor Plan

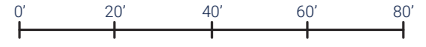



SCALE: 1" = 40'-0" 0' 20' 40' 60' 80'

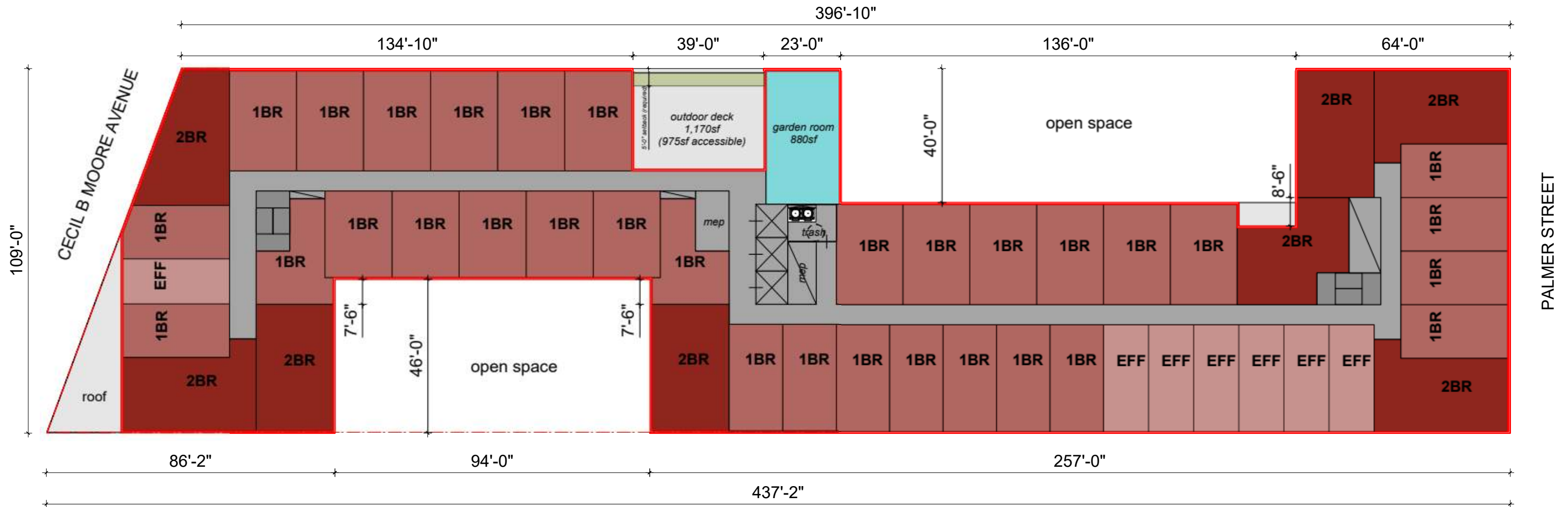


# 2<sup>nd</sup> - 4<sup>th</sup> Floor Plan



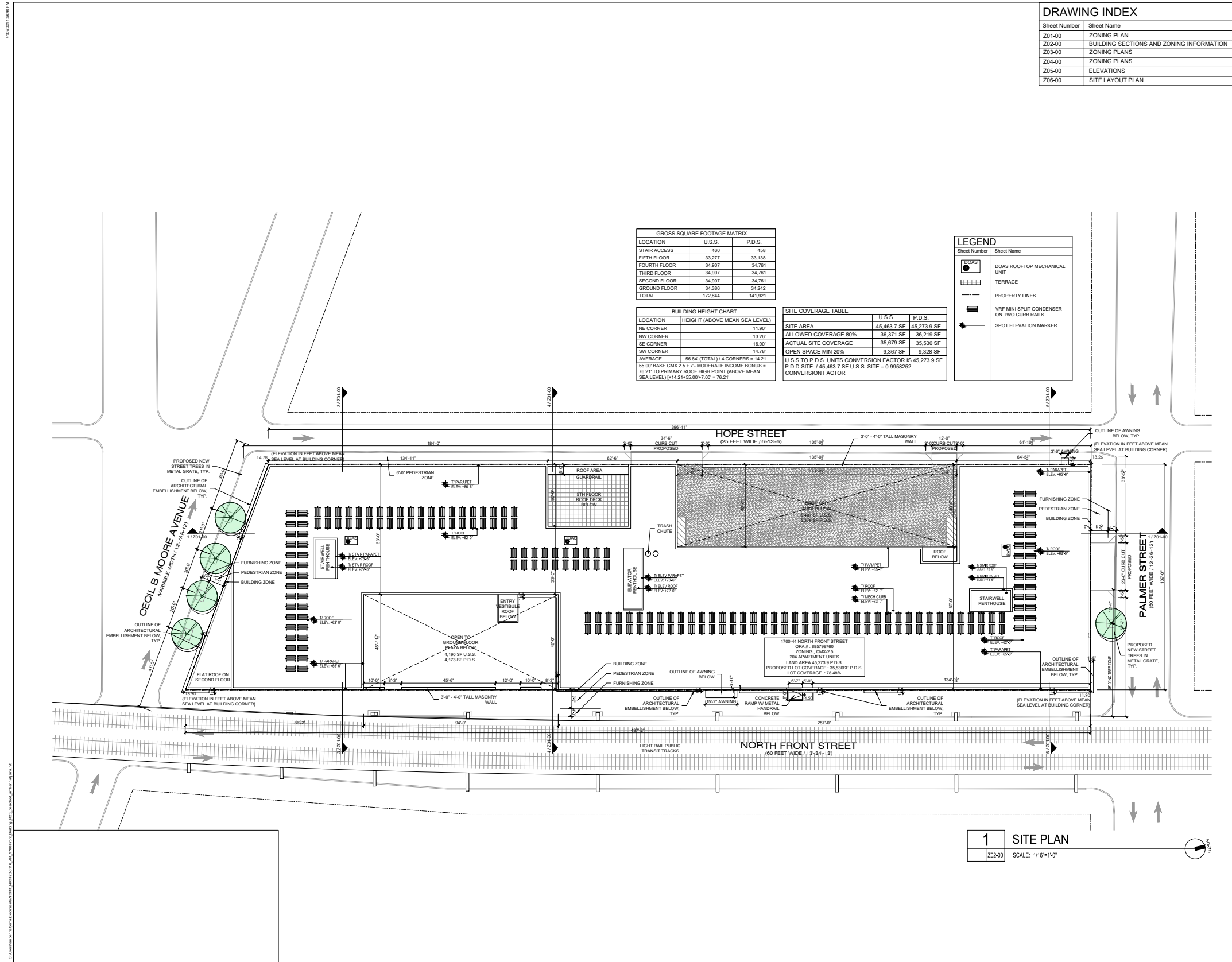
SCALE: 1" = 40'-0"  

# 5th Floor Plan



SCALE: 1" = 40'-0" 0' 20' 40' 60' 80'

# Approved Zoning Plan - For Information Only



DATE	ISSUED FOR	REV
05/26/2021	ZONING SUBMISSION	

This drawing has been prepared solely for the use of the 1700 NORTH FRONT LLC and there are no representations of any kind made by NORR to any party with whom NORR has not entered into a contract. This drawing shall not be used for construction purposes until the seal appearing hereon is signed and dated by the Architect or Engineer.

**Project Component**  
 ISSUE FOR ZONING REVIEW

**Consultants**

**Civil**  
 COLLIER'S ENGINEERING  
 2 Penn Center, Suite 222  
 1500 JFK Boulevard  
 Philadelphia, PA 19102  
 P: 215.861.8021

**Design**  
 CONCRETE  
 Outside Architectural 78a  
 1012 Dr. Amsterdam  
 Netherlands  
 P: 011.31.020.5200.200

**Architect of Record**  
 NORR  
 525 N. La Salle Street, Suite 500  
 Chicago, IL 60654  
 P: 312.424.2400

**Structural**  
 One Penn Center  
 1617 JFK Blvd., Suite 1600  
 Philadelphia, PA 19103

**ME/FP/**  
 NORR  
 One Penn Center  
 1617 JFK Blvd., Suite 1600  
 Philadelphia, PA 19103

**Seals**

**NORR**

One Penn Center  
 1617 JFK Blvd., Suite 1600  
 Philadelphia, PA, US 19103  
 norr.com

**Project Manager**  
 ADAM JECKEL

**Design Lead**  
 NORR

**Client**  
 1700 North Front LLC  
 50 WASHINGTON STREET  
 HOKICKEN, NJ 07030  
 P: 201.963.5200

**Project**  
 1700 N. FRONT ST.  
 1700-44 NORTH FRONT STREET  
 PHILADELPHIA, PA 19122

**Drawing Title**  
 SITE PLAN

**Scale:** As Indicated

**Chicago Project No.** NICH20-0116-00  
**Philadelphia Project No.**

**Drawing No.**  
 Z02-00

ARCH E1 Title Block - R18.3 (Sept 12/19) Copyright © 2021



# Landscape Plan



PLANTING 'A'  
**GLEDITSIA TRIACANTHOS F. INERMIS / SKYLINE HOMEYLOCUST**



PLANTING 'B' **BETULA NIGRA / DURA HEAT RIVER BIRCH**



PLANTING 'C' **LIRIOPE MUSCARI 'VARIEGATA' / VARIEGATED LILYTURF**

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Dong Business with

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Rhett N. Chilberti  
 REGISTERED PROFESSIONAL ENGINEER  
 LICENSE NUMBER: 001234567  
 COLLIERS ENGINEERING & DESIGN, INC.

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CDR RENDERING  
 FOR  
**BARRY URBY LLC**

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**PROJECT TYPE:**  
 MIXED-USE DEVELOPMENT

**LOCATION:** 1700-14 N. FRONT STREET, PHILADELPHIA, PA 19122

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Maser  
 3700 Market Street, Suite 2000  
 Philadelphia, PA 19104  
 Phone: 215.581.1100  
 Fax: 215.581.1101  
 www.maser.com

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DATE	DESCRIPTION	BY	CHKD

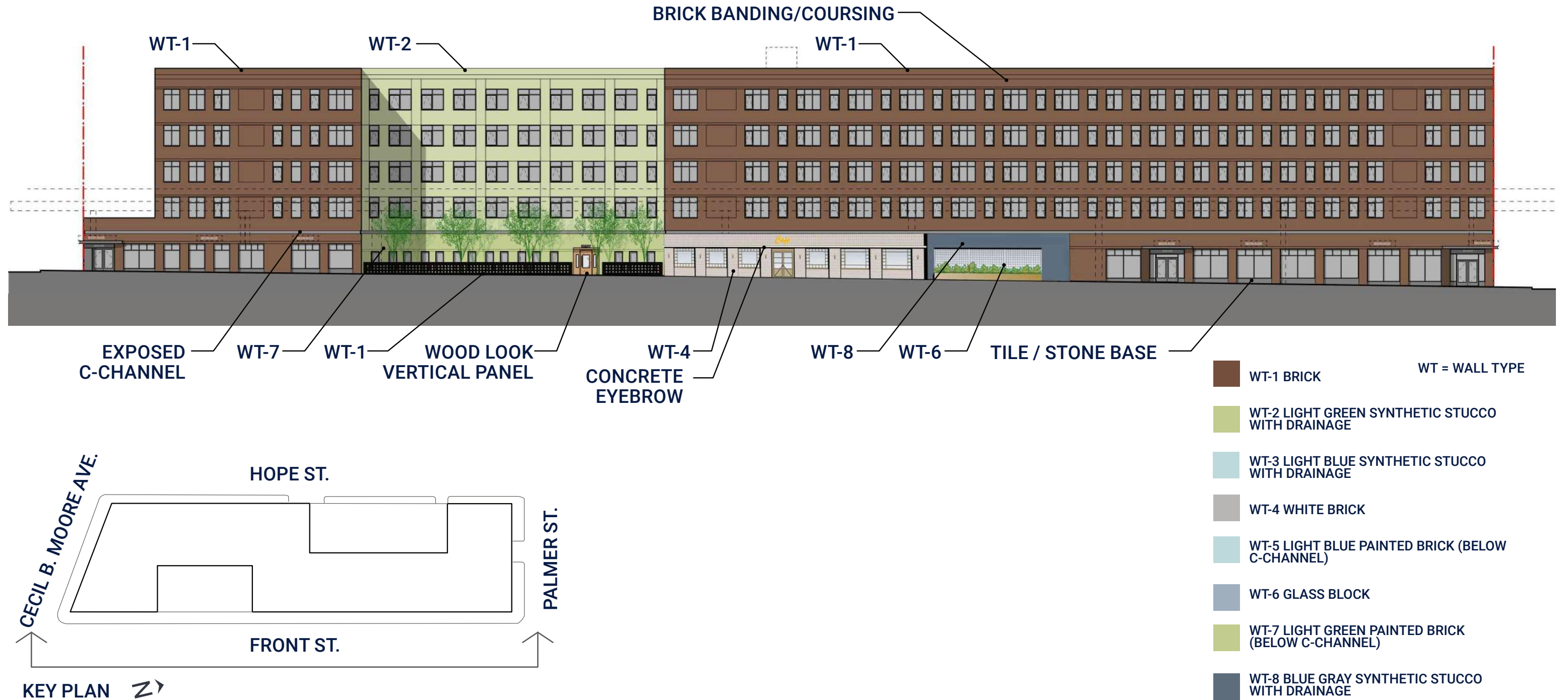
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 SHEET: 1 of 1

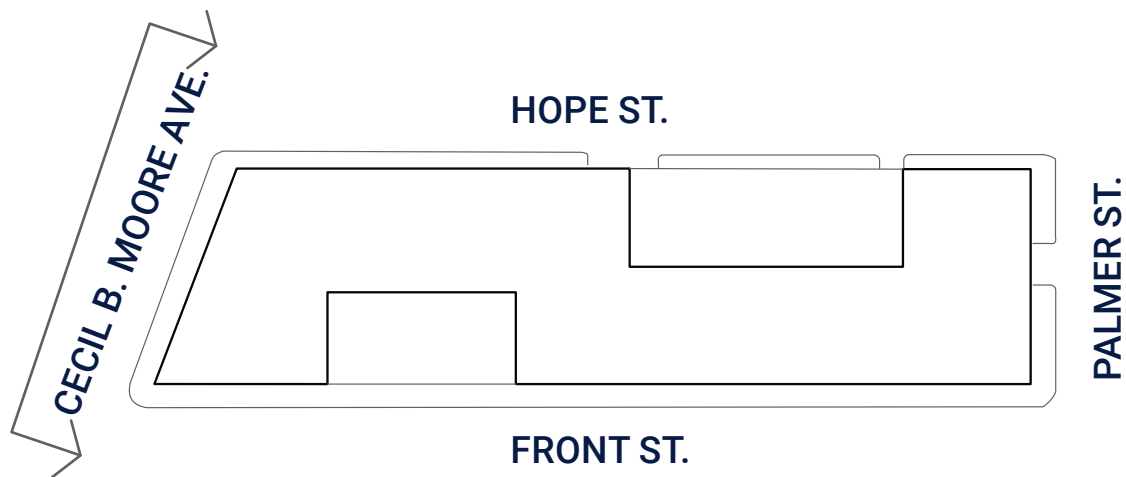
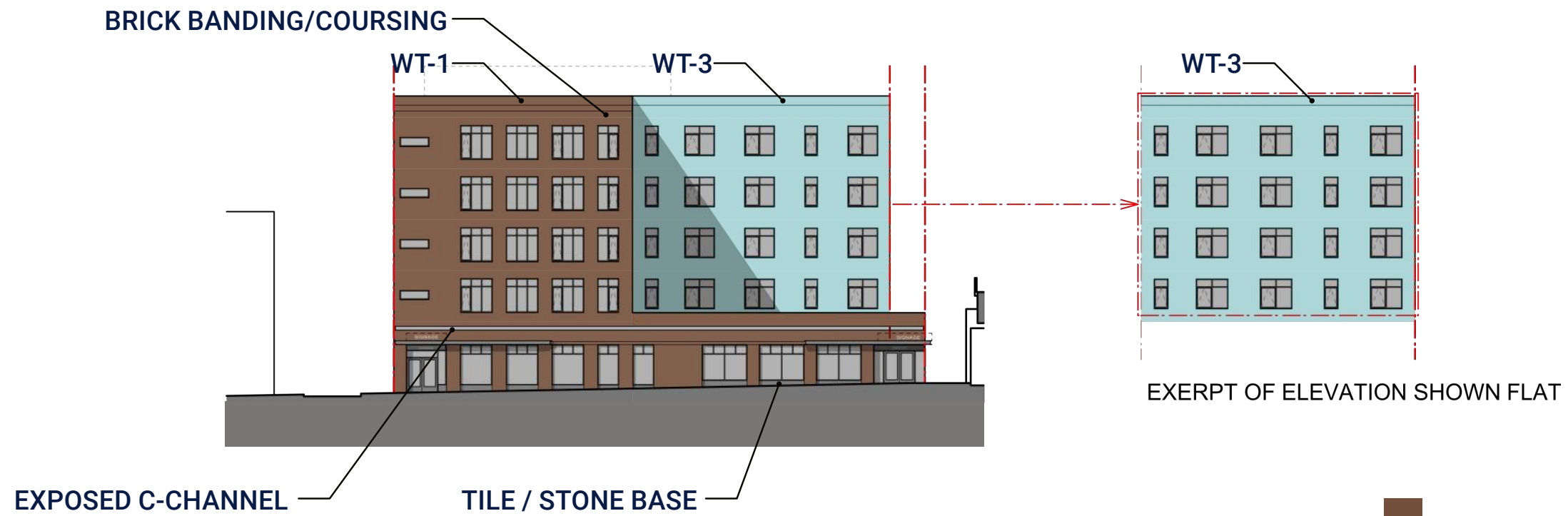
NOTE: DO NOT SCALE DRAWINGS FOR CONSTRUCTION



# N. Front Street Elevation



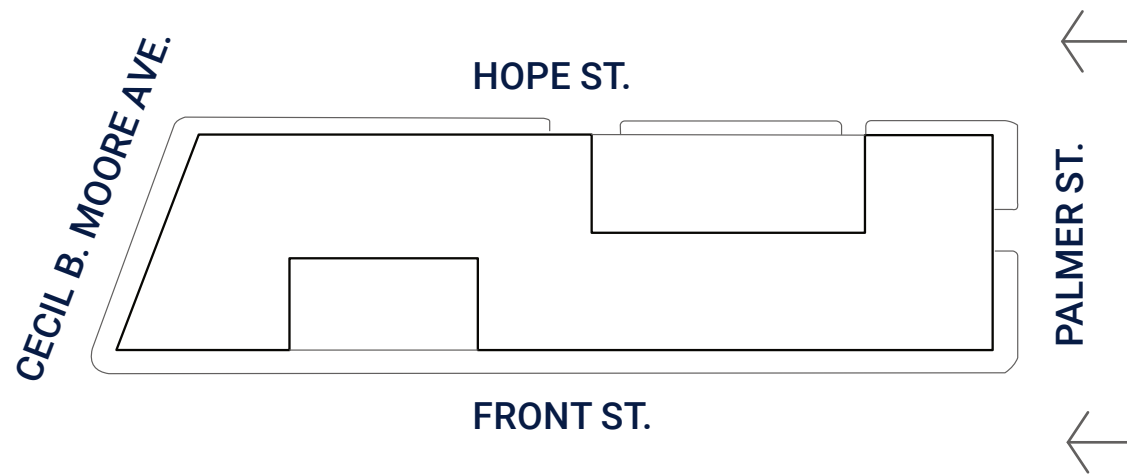
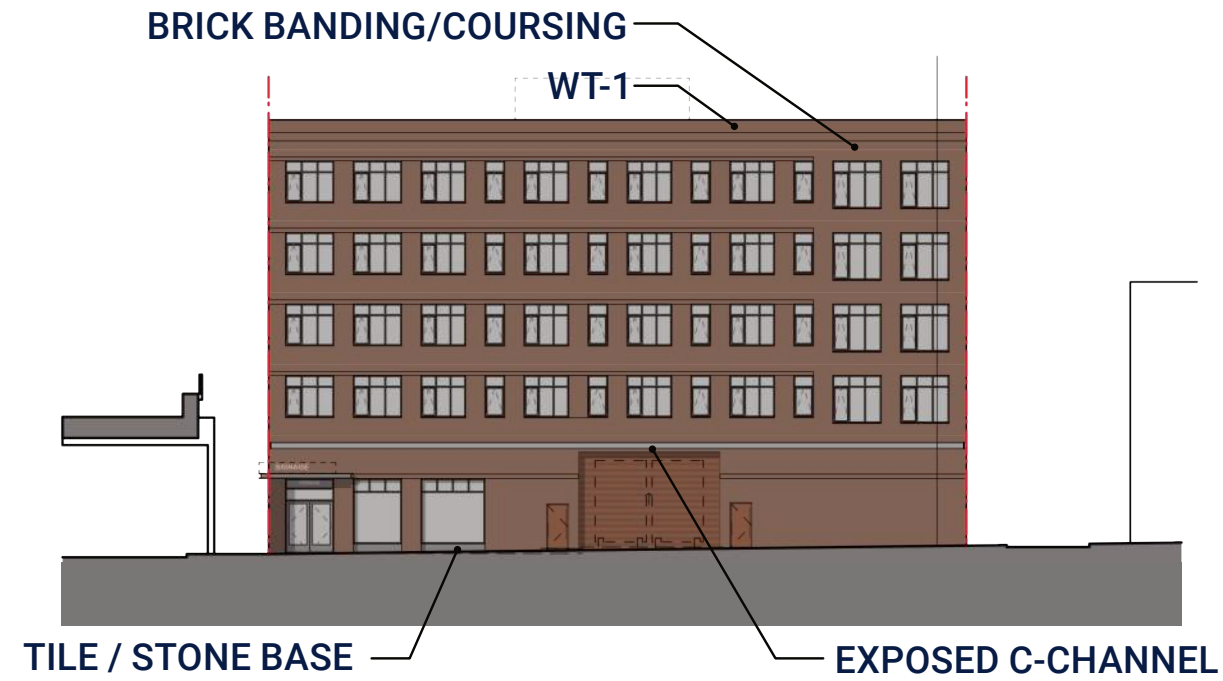
# Cecil B. Moore Avenue Elevation



KEY PLAN ↗

- WT = WALL TYPE
- WT-1 BRICK
  - WT-2 LIGHT GREEN SYNTHETIC STUCCO WITH DRAINAGE
  - WT-3 LIGHT BLUE SYNTHETIC STUCCO WITH DRAINAGE
  - WT-4 WHITE BRICK
  - WT-5 LIGHT BLUE PAINTED BRICK (BELOW C-CHANNEL)
  - WT-6 GLASS BLOCK
  - WT-7 LIGHT GREEN PAINTED BRICK (BELOW C CHANNEL)
  - WT-8 LIGHT PURPLE SYNTHETIC STUCCO WITH DRAINAGE

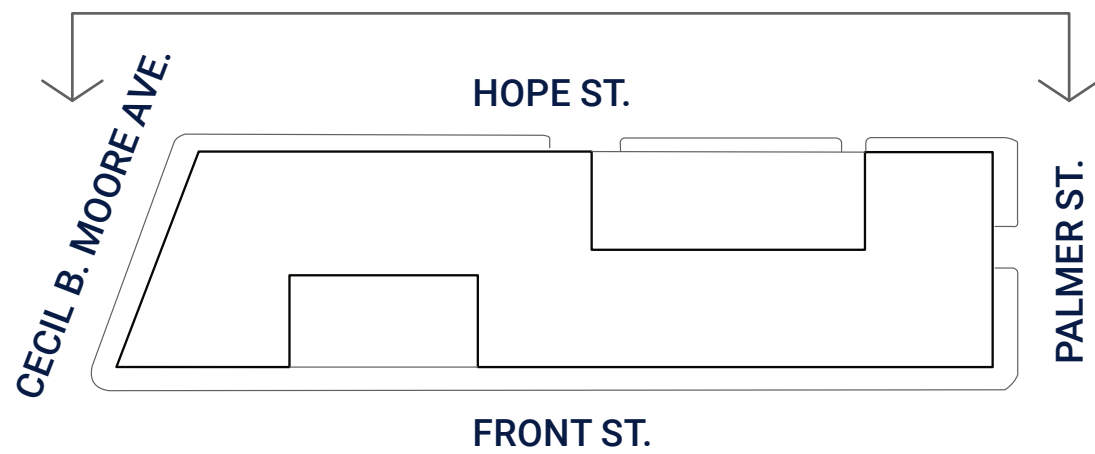
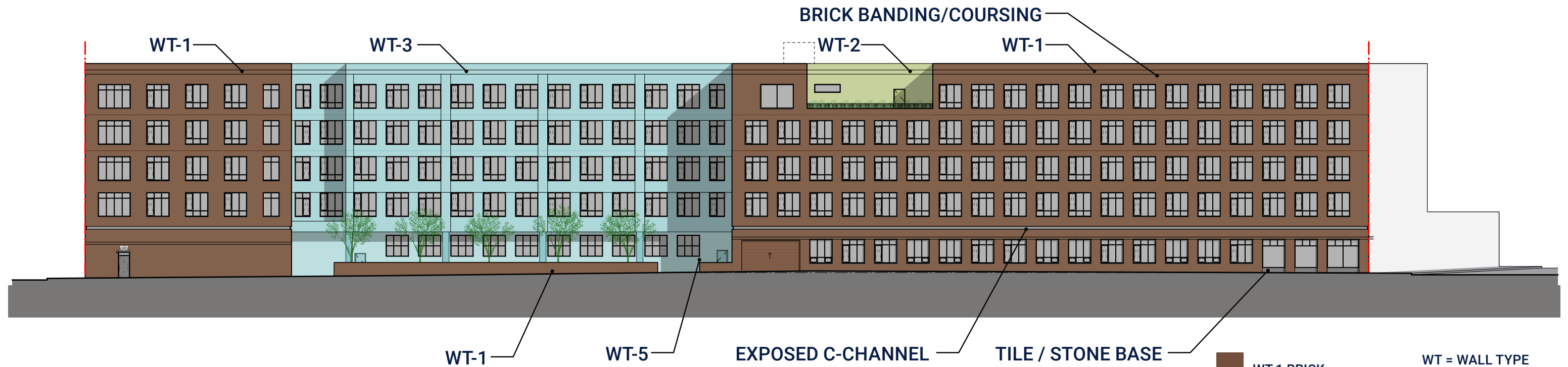
# Palmer Street Elevation



KEY PLAN ↗

- WT = WALL TYPE
- WT-1 BRICK
  - WT-2 LIGHT GREEN SYNTHETIC STUCCO WITH DRAINAGE
  - WT-3 LIGHT BLUE SYNTHETIC STUCCO WITH DRAINAGE
  - WT-4 WHITE BRICK
  - WT-5 LIGHT BLUE PAINTED BRICK (BELOW C-CHANNEL)
  - WT-6 GLASS BLOCK
  - WT-7 LIGHT GREEN PAINTED BRICK (BELOW C-CHANNEL)
  - WT-8 LIGHT PURPLE SYNTHETIC STUCCO WITH DRAINAGE

# Hope Street Elevation

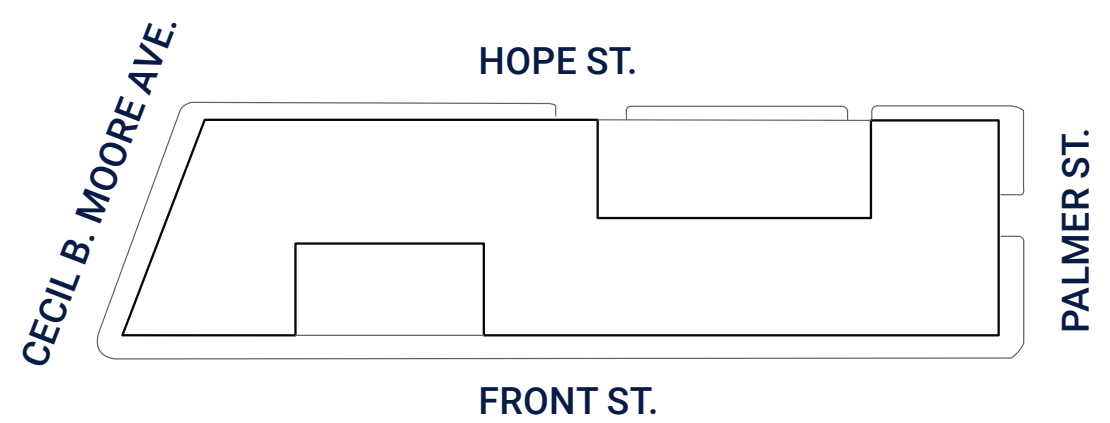
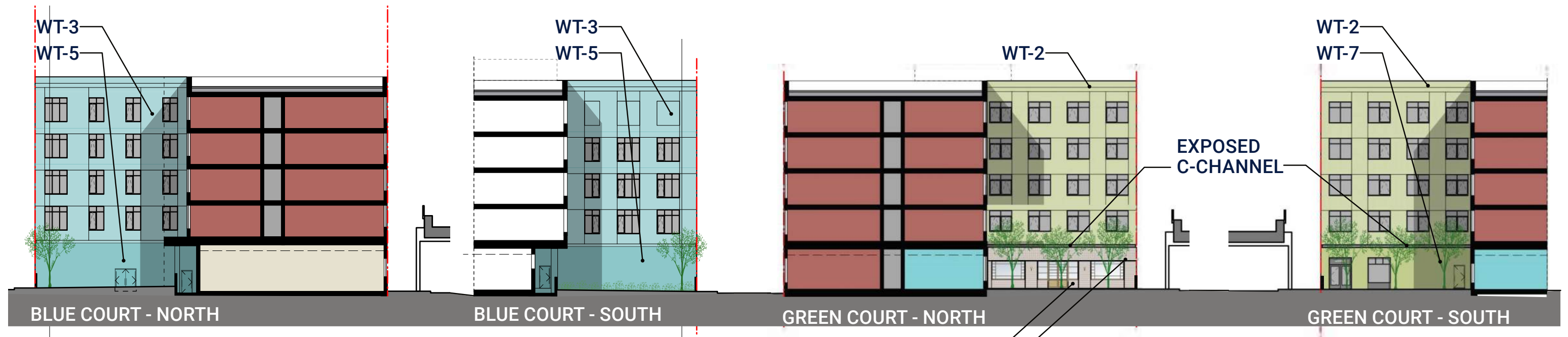


KEY PLAN 

- WT = WALL TYPE
-  WT-1 BRICK
  -  WT-2 LIGHT GREEN SYNTHETIC STUCCO WITH DRAINAGE
  -  WT-3 LIGHT BLUE SYNTHETIC STUCCO WITH DRAINAGE
  -  WT-4 WHITE BRICK
  -  WT-5 LIGHT BLUE PAINTED BRICK (BELOW C-CHANNEL)
  -  WT-6 GLASS BLOCK
  -  WT-7 LIGHT GREEN PAINTED BRICK (BELOW C-CHANNEL)
  -  WT-8 LIGHT PURPLE SYNTHETIC STUCCO WITH DRAINAGE



# Courtyard Elevations



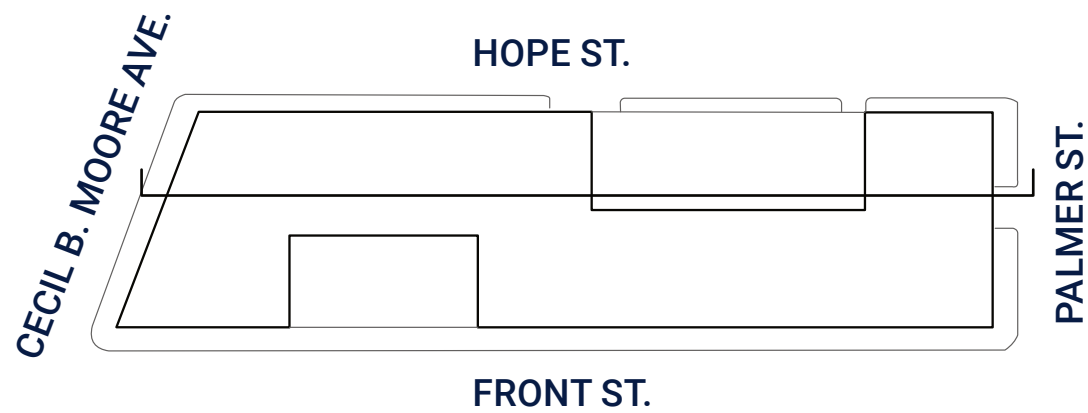
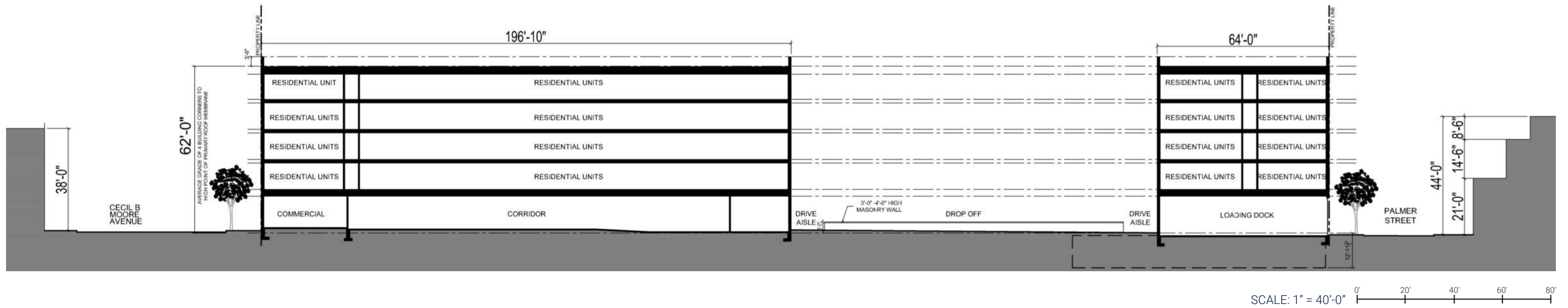
KEY PLAN

- WT = WALL TYPE
- WT-1 BRICK
  - WT-2 LIGHT GREEN SYNTHETIC STUCCO WITH DRAINAGE
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  - WT-4 WHITE BRICK
  - WT-5 LIGHT BLUE PAINTED BRICK (BELOW C-CHANNEL)
  - WT-6 GLASS BLOCK
  - WT-7 LIGHT GREEN PAINTED BRICK (BELOW C-CHANNEL)
  - WT-8 LIGHT PURPLE SYNTHETIC STUCCO WITH DRAINAGE

# Building Materials

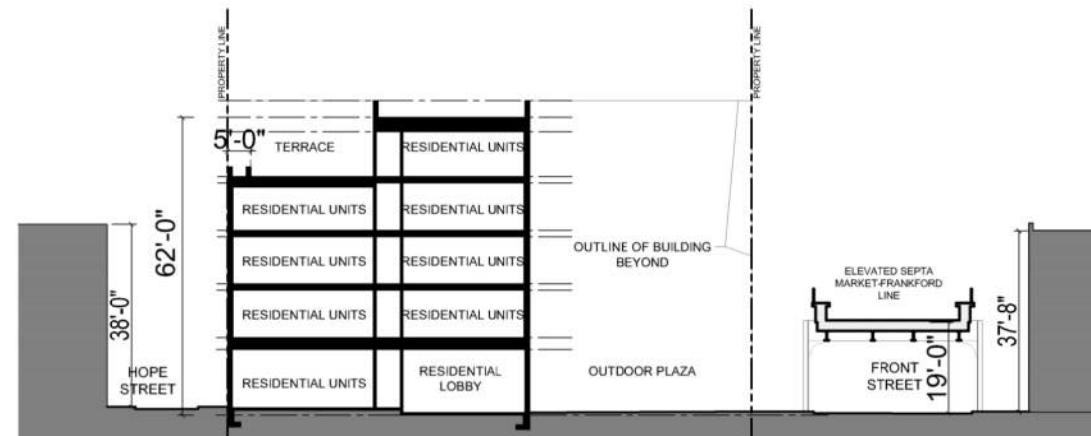


# Site Sections

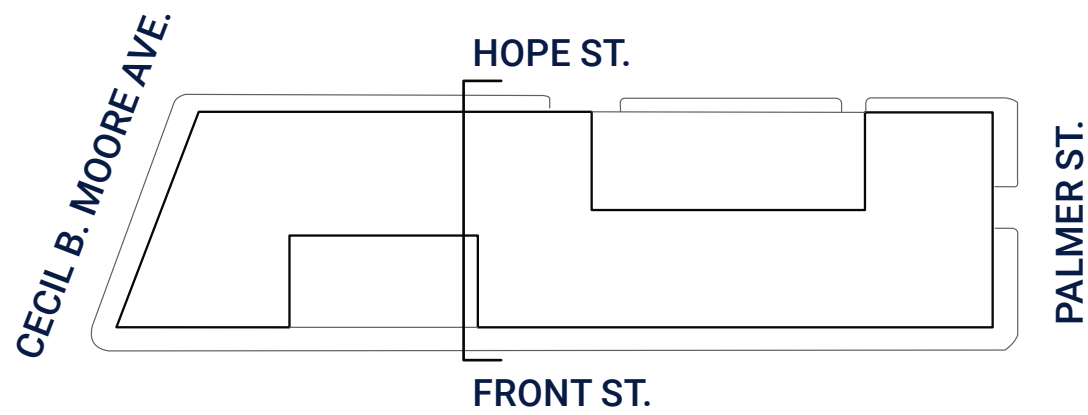


KEY PLAN 

# Site Sections



SCALE: 1" = 40'-0" 0' 20' 40' 60' 80'



KEY PLAN 

# Exterior Rendering - Cecil B. Moore St. And N. Front St.



# Exterior Rendering - Cecil B. Moore St.



# Aerial Rendering - Cecil B. Moore St. and Hope St.



# Aerial Rendering - Cecil B. Moore St. And N. Front St.





# Sustainability Questionnaire

Categories	Benchmark	Does project meet benchmark? If yes, please explain how. If no, please explain why not.
<b>Location and Transportation</b>		
(1) Access to Quality Transit	Locate a functional entry of the project within a ¼-mile (400-meter) walking distance of existing or planned bus, streetcar, or rideshare stops, bus rapid transit stops, light or heavy rail stations.	Yes, the main entry door to the lobby is 1,200 feet (365 meters) from the Berks St MFL elevated station and Berks St & Front St bus stop.
(2) Reduced Parking Footprint	All new parking areas will be in the rear yard of the property or under the building, and unenclosed or uncovered parking areas are 40% or less of the site area.	On-site parking is not required or provided. A paved drop-off zone is provided on the Hope Street side of the building with a corridor from this area to the lobby. The total open areas on site are roughly 20%.
(3) Green Vehicles	Designate 5% of all parking spaces used by the project as preferred parking for green vehicles or car share vehicles. Clearly identify and enforce for sole use by car share or green vehicles, which include plug-in electric vehicles and alternative fuel vehicles.	On-site parking is not required or provided.
(4) Railway Setbacks (Excluding frontages facing trolleys/light rail or enclosed subsurface rail lines or subways)	To foster safety and maintain a quality of life protected from excessive noise and vibration, residential development with railway frontages should be setback from rail lines and the building's exterior envelope, including windows, should reduce exterior sound transmission to 60dBA. (If setback used, specify distance) <sup>1</sup>	437' total building linear feet is along N Front Street. 94' linear feet are setback approximately 46' from the property line. 343' linear feet of the North Front Street facade is built to the property line. 22' linear feet is only 1-story in height. The owner will be looking to use higher OITC windows in addition to engaging an acoustical engineer for recommendations to reduce the exterior sound transmission.
(5) Bike Share Station	Incorporate a bike share station in coordination with and conformance to the standards of Philadelphia Bike Share.	No. On-site bicycle parking storage at grade with a door directly to the sidewalk is provided for tenants.
<b>Sustainable Sites</b>		
(7) Pervious Site Surfaces	Provides vegetated and/or pervious open space that is 30% or greater of the site's Open Area, as defined by the zoning code. Vegetated and/or green roofs can be included in this calculation.	The roughly 9,590 SF U.S.S. Open Area has an area of roughly 2,200 SF that is vegetated at grade. The green roof area is 700 SF. The total 2,900 SF is 30% of the 9,590 SF Open Area.
(8) Rainwater Management	Conform to the stormwater requirements of the Philadelphia Water Department(PWD) and either: A) Develop a green street and donate it to PWD, designed and constructed in accordance with the PWD Green Streets Design Manual, OR B) Manage additional runoff from adjacent streets on the development site, designed and constructed in accordance with specifications of the PWD Stormwater Management Regulations	The site is not proposing to develop a green street or manage off-site stormwater.  All on-site stormwater will be managed with a combination of green roof areas and subsurface detention tank, which conforms to PWD regulations.
(9) Heat Island Reduction (excluding roofs)	Reduce the heat island effect through either of the following strategies for 50% or more of all on-site hardscapes: A) Hardscapes that have a high reflectance, an SRI>29. B) Shading by trees, structures, or solar panels.	Yes, the site is proposing trees and light color compacted gravel in the North Front Street landscapes courtyard which shall reduce the heat island effect.

Categories	Benchmark	Does project meet benchmark? If yes, please explain how. If no, please explain why not.
<b>Water Efficiency</b>		
(6) Outdoor Water Use	Maintain on-site vegetation without irrigation. OR, Reduce of watering requirements at least 50% from the calculated baseline for the site's peak watering month.	We will avoid on-site irrigation to the maximum extent possible such as using sedum tray with native species for green roof areas, but the N Front St courtyard will require some irrigation.
<b>Energy and Atmosphere</b>		
(10) Energy Commissioning and Energy Performance - Adherence to the New Building Code	PCPC notes that as of April 1, 2019 new energy conservation standards are required in the Philadelphia Building Code, based on recent updates of the International Energy Conservation Code (IECC) and the option to use ASHRAE 90.01-2016. PCPC staff asks the applicant to state which path they are taking for compliance, including their choice of code and any options being pursued under the 2018 IECC. <sup>ii</sup>	The project shall comply with 2018 IECC and will utilize the prescriptive compliance method and COMcheck.
(11) Energy Commissioning and Energy Performance - Going beyond the code	Will the project pursue energy performance measures beyond what is required in the Philadelphia code by meeting any of these benchmarks? <sup>iii</sup> •Reduce energy consumption by achieving 10% energy savings or more from an established baseline using	No. Energy consumption will be reduced by use of LED lighting and efficient HVAC units.
	ASHRAE standard 90.1-2016 (LEED v4.1 metric). •Achieve certification in Energy Star for Multifamily New Construction (MFNC). •Achieve Passive House Certification	
(12) Indoor Air Quality and Transportation	Any sites within 1000 feet of an interstate highway, state highway, or freeway will provide air filters for all regularly occupied spaces that have a Minimum Efficiency Reporting Value (MERV) of 13. Filters shall be installed prior to occupancy. <sup>iv</sup>	The site is not within 1,000 feet of an interstate highway, state highway, or freeway.
(13) On-Site Renewable Energy	Produce renewable energy on-site that will provide at least 3% of the project's anticipated energy usage.	Not provided.
<b>Innovation</b>		
(14) Innovation	Any other sustainable measures that could positively impact the public realm.	No

# Complete Street Handbook Checklist

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### INSTRUCTIONS

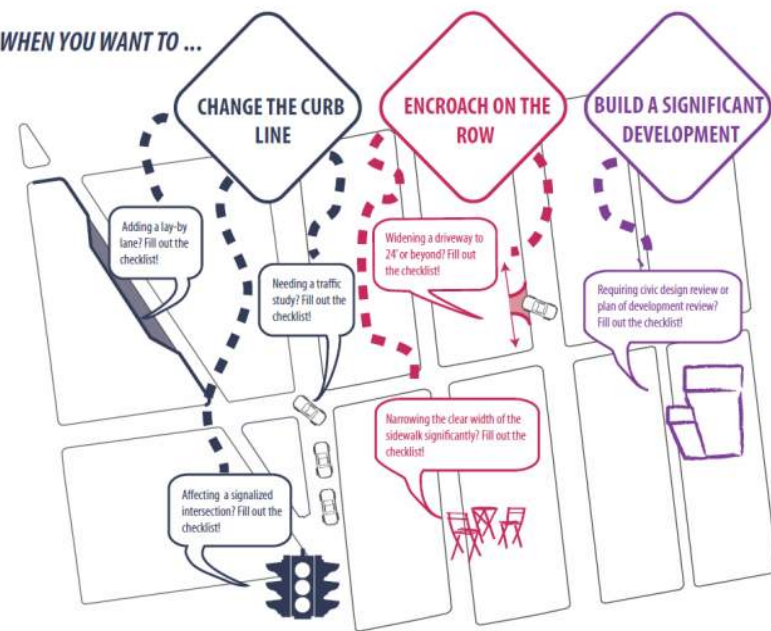
This Checklist is an implementation tool of the *Philadelphia Complete Streets Handbook* (the “Handbook”) and enables City engineers and planners to review projects for their compliance with the Handbook’s policies. The handbook provides design guidance and does not supersede or replace language, standards or policies established in the City Code, City Plan, or Manual on Uniform Traffic Control Devices (MUTCD).

The Philadelphia City Planning Commission receives this Checklist as a function of its Civic Design Review (CDR) process. This checklist is used to document how project applicants considered and accommodated the needs of all users of city streets and sidewalks during the planning and/or design of projects affecting public rights-of-way. Departmental reviewers will use this checklist to confirm that submitted designs incorporate complete streets considerations (see §11-901 of The Philadelphia Code). Applicants for projects that require Civic Design Review shall complete this checklist and attach it to plans submitted to the Philadelphia City Planning Commission for review, along with an electronic version.

The Handbook and the checklist can be accessed at <http://www.phila.gov/CityPlanning/projectreviews/Pages/CivicDesignReview.aspx>

### WHEN DO I NEED TO FILL OUT THE COMPLETE STREETS CHECKLIST?

WHEN YOU WANT TO ...



PRELIMINARY PCPC REVIEW AND COMMENT:

DATE

FINAL STREETS DEPT REVIEW AND COMMENT:

DATE

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### INSTRUCTIONS (continued)

APPLICANTS SHOULD MAKE SURE TO COMPLY WITH THE FOLLOWING REQUIREMENTS:

- This checklist is designed to be filled out electronically in Microsoft Word format. Please submit the Word version of the checklist. Text fields will expand automatically as you type.
- All plans submitted for review must clearly dimension the widths of the Furnishing, Walking, and Building Zones (as defined in Section 1 of the Handbook). “High Priority” Complete Streets treatments (identified in Table 1 and subsequent sections of the Handbook) should be identified and dimensioned on plans.
- All plans submitted for review must clearly identify and site all street furniture, including but not limited to bus shelters, street signs and hydrants.
- Any project that calls for the development and installation of medians, bio-swales and other such features in the right-of-way may require a maintenance agreement with the Streets Department.
- ADA curb-ramp designs must be submitted to Streets Department for review
- Any project that significantly changes the curb line may require a City Plan Action. The City Plan Action Application is available at <http://www.philadelphiastreet.com/survey-and-design-bureau/city-plans-unit>. An application to the Streets Department for a City Plan Action is required when a project plan proposes the:
  - o Placing of a new street;
  - o Removal of an existing street;
  - o Changes to roadway grades, curb lines, or widths; or
  - o Placing or striking a city utility right-of-way.

Complete Streets Review Submission Requirement\*:

- EXISTING CONDITIONS SITE PLAN, should be at an identified standard engineering scale
  - o FULLY DIMENSIONED
  - o CURB CUTS/DRIVEWAYS/LAYBY LANES
  - o TREE PITS/LANDSCAPING
  - o BICYCLE RACKS/STATIONS/STORAGE AREAS
  - o TRANSIT SHELTERS/STAIRWAYS
- PROPOSED CONDITIONS SITE PLAN, should be at an identified standard engineering scale
  - o FULLY DIMENSIONED, INCLUDING DELINEATION OF WALKING, FURNISHING, AND BUILDING ZONES AND PINCH POINTS
  - o PROPOSED CURB CUTS/DRIVEWAYS/LAYBY LANES
  - o PROPOSED TREE PITS/LANDSCAPING
  - o BICYCLE RACKS/STATIONS/STORAGE AREAS
  - o TRANSIT SHELTERS/STAIRWAYS

\*APPLICANTS PLEASE NOTE: ONLY FULL-SIZE, READABLE SITE PLANS WILL BE ACCEPTED. ADDITIONAL PLANS MAY BE REQUIRED AND WILL BE REQUESTED IF NECESSARY

# Complete Street Handbook Checklist

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### GENERAL PROJECT INFORMATION

- |   |  |
|---|--|
| <p>1. PROJECT NAME<br/><u>1700 N Front Street</u></p> <p>3. APPLICANT NAME<br/><u>1700 North Front Street LLC</u></p> <p>4. APPLICANT CONTACT INFORMATION<br/><u>1824 Spruce Street, Suite 100 Philadelphia, PA 19103</u></p> <p>6. OWNER NAME<br/><u>1700 North Front Street LLC</u></p> <p>7. OWNER CONTACT INFORMATION<br/><u>Dennis Giuliano dgiuliano@urby.com</u></p> <p>8. ENGINEER / ARCHITECT NAME<br/><u>Colliers Engineering, Rhett Chiliberti, PE</u><br/><u>NORR, William Westhafer, AIA</u><br/><u>NORR, Adam Jeckel, AIA</u></p> <p>9. ENGINEER / ARCHITECT CONTACT INFORMATION<br/><u>Colliers Engineering, 2 Penn Center</u><br/><u>Suite 222, Philadelphia, PA 19102</u><br/><a href="mailto:rhett.chiliberti@colliersengineering.com">rhett.chiliberti@colliersengineering.com</a><br/><a href="mailto:max.bustos@colliersengineering.com">max.bustos@colliersengineering.com</a><br/><u>NORR, One Penn Center</u><br/><u>1617 JFL Blvd, Suite 1600, Philadelphia PA 19103</u><br/><a href="mailto:william.westhafer@norr.com">william.westhafer@norr.com</a><br/><a href="mailto:adam.jeckel@norr.com">adam.jeckel@norr.com</a></p> | <p>2. DATE<br/><u>June 10, 2021</u></p> <p>5. PROJECT AREA: list precise street limits and scope<br/><u>On-site disturbance: 43,000 SF U.S.S.</u><br/><u>Right-of-way disturbance: SF U.S.S.</u></p> |
|---|--|

10. STREETS: List the streets associated with the project. Complete Streets Types can be found at [www.phila.gov/map](http://www.phila.gov/map) under the "Complete Street Types" field. Complete Streets Types are also identified in Section 3 of the Handbook.

Also available here: <http://metadata.phila.gov/#home/datasetdetails/5543867320583086178c4f34/>

STREET	FROM	TO	COMPLETE STREET TYPE
<u>N Front Street</u>	<u>Cecil B. Moore Ave</u>	<u>W Palmer Street</u>	<u>Urban Arterial</u>
<u>Cecil B. Moore Ave</u>	<u>N Front Street</u>	<u>Hope Street</u>	<u>Urban Arterial</u>
<u>Palmer Street</u>	<u>N Front Street</u>	<u>Hope Street</u>	<u>Local</u>
<u>Hope Street</u>	<u>Cecil B. Moore Ave</u>	<u>W Palmer Street</u>	<u>Local</u>

11. Does the **Existing Conditions** site survey clearly identify the following existing conditions with dimensions?
- |   |   |                             |                              |
|---|---|-----------------------------|------------------------------|
| a. Parking and loading regulations in curb lanes adjacent to the site | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> |                              |
| b. Street Furniture such as bus shelters, honor boxes, etc.           | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> |
| c. Street Direction   | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> |                              |
| d. Curb Cuts  | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> |

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



- |   |   |                             |   |
|---|---|-----------------------------|---|
| e. Utilities, including tree grates, vault covers, manholes, junction boxes, signs, lights, poles, etc. | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/>            |
| f. Building Extensions into the sidewalk, such as stairs and stoops                                     | YES <input type="checkbox"/>            | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> |

### APPLICANT: General Project Information

Additional Explanation / Comments: \_\_\_\_\_

### DEPARTMENTAL REVIEW: General Project Information

# Complete Street Handbook Checklist

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### PEDESTRIAN COMPONENT (Handbook Section 4.3)

12. SIDEWALK: list Sidewalk widths for each street frontage. Required Sidewalk widths are listed in Section 4.3 of the Handbook.

STREET FRONTAGE	TYPICAL SIDEWALK WIDTH (BUILDING LINE TO CURB)	CITY PLAN SIDEWALK WIDTH
	Required / Existing / Proposed	Existing / Proposed
<b>Cecil B. Moore Avenue</b>	<u>12'-5" / Varies / 12'</u>	<u>12'-5" / 12'-5"</u>
<b>N Front Street</b>	<u>13' / 34' / 13'</u>	<u>13' / 13'</u>
<b>W Palmer Street</b>	<u>12' / 26' / 12'</u>	<u>12' / 12'</u>
<b>Hope Street</b>	<u>6' / 13' / 6'</u>	<u>6' / 6'</u>

13. WALKING ZONE: list Walking Zone widths for each street frontage. The Walking Zone is defined in Section 4.3 of the Handbook, including required widths.

STREET FRONTAGE	WALKING ZONE
	Required / Existing / Proposed
<b>Cecil B. Moore Avenue</b>	<u>6'-0" / 7'-5" / 7'-5"</u>
<b>N Front Street</b>	<u>6'-0" / 9' / 9'-0"</u>
<b>Palmer Street</b>	<u>5'-0" / 8' / 8'-0"</u>
<b>Hope Street</b>	<u>5'-0" / 6' / 6'-0"</u>

14. VEHICULAR INTRUSIONS: list Vehicular Intrusions into the sidewalk. Examples include but are not limited to; driveways, lay-by lanes, etc. Driveways and lay-by lanes are addressed in sections 4.8.1 and 4.6.3, respectively, of the Handbook.

#### EXISTING VEHICULAR INTRUSIONS

INTRUSION TYPE	INTRUSION WIDTH	PLACEMENT
<u>Driveway/curb cut</u>	<u>33'-2"</u>	<u>N Front Street</u>

#### PROPOSED VEHICULAR INTRUSIONS

INTRUSION TYPE	INTRUSION WIDTH	PLACEMENT
<u>Driveway/curb cut</u>	<u>23'-0"</u>	<u>W Palmer Street</u>
<u>Driveway/curb cut</u>	<u>34'-6"</u>	<u>Hope Street</u>
<u>Driveway/curb cut</u>	<u>12'-0"</u>	<u>Hope Street</u>

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### PEDESTRIAN COMPONENT (continued)

15. When considering the overall design, does it create or enhance a pedestrian environment that provides safe and comfortable access for all pedestrians at all times of the day? YES  NO

DEPARTMENTAL APPROVAL  
YES  NO

APPLICANT: Pedestrian Component  
Additional Explanation / Comments: \_\_\_\_\_

DEPARTMENTAL REVIEW: Pedestrian Component  
Reviewer Comments: \_\_\_\_\_

# Complete Street Handbook Checklist

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### BUILDING & FURNISHING COMPONENT (Handbook Section 4.4)

16. BUILDING ZONE: list the MAXIMUM, existing and proposed Building Zone width on each street frontage. The Building Zone is defined as the area of the sidewalk immediately adjacent to the building face, wall, or fence marking the property line, or a lawn in lower density residential neighborhoods. The Building Zone is further defined in section 4.4.1 of the Handbook.

STREET FRONTAGE	MAXIMUM BUILDING ZONE WIDTH Existing / Proposed
<u>Cecil B. Moore Avenue</u>	<u>No minimum / 0'</u>
<u>N Front Street</u>	<u>No minimum / 5' (ramp entry to Café)</u>
<u>Palmer Street</u>	<u>No minimum / 0'</u>
<u>Hope Street</u>	<u>No minimum / 0'</u>

17. FURNISHING ZONE: list the MINIMUM, recommended, existing, and proposed Furnishing Zone widths on each street frontage. The Furnishing Zone is further defined in section 4.4.2 of the Handbook.

STREET FRONTAGE	MINIMUM FURNISHING ZONE WIDTH Recommended / Existing / Proposed
<u>Cecil B. Moore Avenue</u>	<u>4' / 4' / 4'</u>
<u>N Front Street</u>	<u>4' / 4' / 4'</u>
<u>Palmer Street</u>	<u>3.5' / 4' / 4'</u>
<u>Hope Street</u>	<u>3.5' / 0' / 0'</u>

18. Identify proposed "high priority" building and furnishing zone design treatments that are incorporated into the design plan, where width permits (see Handbook Table 1). Are the following treatments identified and dimensioned on the plan?

- |                    |   |                             |   |                       |                              |                             |
|--------------------|---|-----------------------------|---|-----------------------|------------------------------|-----------------------------|
| ▪ Bicycle Parking  | YES <input type="checkbox"/>            | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | DEPARTMENTAL APPROVAL | YES <input type="checkbox"/> | NO <input type="checkbox"/> |
| ▪ Lighting         | YES <input type="checkbox"/>            | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | DEPARTMENTAL APPROVAL | YES <input type="checkbox"/> | NO <input type="checkbox"/> |
| ▪ Benches          | YES <input type="checkbox"/>            | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | DEPARTMENTAL APPROVAL | YES <input type="checkbox"/> | NO <input type="checkbox"/> |
| ▪ Street Trees     | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/>            | DEPARTMENTAL APPROVAL | YES <input type="checkbox"/> | NO <input type="checkbox"/> |
| ▪ Street Furniture | YES <input type="checkbox"/>            | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | DEPARTMENTAL APPROVAL | YES <input type="checkbox"/> | NO <input type="checkbox"/> |

19. Does the design avoid tripping hazards? YES  NO  N/A  DEPARTMENTAL APPROVAL YES  NO

20. Does the design avoid pinch points? Pinch points are locations where the Walking Zone width is less than the required width identified in item 13, or requires an exception YES  NO  N/A  DEPARTMENTAL APPROVAL YES  NO

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### BUILDING & FURNISHING COMPONENT (continued)

21. Do street trees and/or plants comply with street installation requirements (see sections 4.4.7 & 4.4.8) YES  NO  N/A  DEPARTMENTAL APPROVAL YES  NO
22. Does the design maintain adequate visibility for all roadway users at intersections? YES  NO  N/A  DEPARTMENTAL APPROVAL YES  NO

**APPLICANT: Building & Furnishing Component**  
Additional Explanation / Comments: The existing Hope Street sidewalk is extremely narrow and difficult to comply with the required walking and furnishing zones. Also, the placement of Street Trees is not shown on Hope Street due to the existing width of Hope Street and conflicts with existing utilities.

**DEPARTMENTAL REVIEW: Building & Furnishing Component**  
Reviewer Comments:

# Complete Street Handbook Checklist

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### BICYCLE COMPONENT (Handbook Section 4.5)

23. List elements of the project that incorporate recommendations of the Pedestrian and Bicycle Plan, located online at <http://phila2035.org/wp-content/uploads/2012/06/bikePedfinal2.pdf>  
N/A

24. List the existing and proposed number of bicycle parking spaces, on- and off-street. Bicycle parking requirements are provided in The Philadelphia Code, Section 14-804.

BUILDING / ADDRESS	REQUIRED SPACES	ON-STREET Existing / Proposed	ON SIDEWALK Existing / Proposed	OFF-STREET Existing / Proposed
<b>1700 N Front Street</b>	<b>70</b>	<b>0 / 0</b>	<b>0 / 0</b>	<b>0 / 70</b>
_____	_____	____/____	____/____	____/____
_____	_____	____/____	____/____	____/____
_____	_____	____/____	____/____	____/____

25. Identify proposed "high priority" bicycle design treatments (see Handbook Table 1) that are incorporated into the design plan, where width permits. Are the following "High Priority" elements identified and dimensioned on the plan?

<ul style="list-style-type: none"> <li>▪ Conventional Bike Lane</li> <li>▪ Buffered Bike Lane</li> <li>▪ Bicycle-Friendly Street</li> <li>▪ Indego Bicycle Share Station</li> </ul>	YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
---	--	--	--	--

26. Does the design provide bicycle connections to local bicycle, trail, and transit networks? YES  NO  N/A

27. Does the design provide convenient bicycle connections to residences, work places, and other destinations? YES  NO  N/A

**APPLICANT: Bicycle Component**  
Additional Explanation / Comments: \_\_\_\_\_

**DEPARTMENTAL REVIEW: Bicycle Component**  
Reviewer Comments:

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### CURBSIDE MANAGEMENT COMPONENT (Handbook Section 4.6)

28. Does the design limit conflict among transportation modes along the curb?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>	DEPARTMENTAL APPROVAL YES <input type="checkbox"/> NO <input type="checkbox"/>
29. Does the design connect transit stops to the surrounding pedestrian network and destinations?	YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
30. Does the design provide a buffer between the roadway and pedestrian traffic?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
31. How does the proposed plan affect the accessibility, visibility, connectivity, and/or attractiveness of public transit?		YES <input type="checkbox"/> NO <input type="checkbox"/>

**APPLICANT: Curbside Management Component**  
Additional Explanation / Comments: \_\_\_\_\_

**DEPARTMENTAL REVIEW: Curbside Management Component**  
Reviewer Comments:

# Complete Street Handbook Checklist

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### VEHICLE / CARTWAY COMPONENT (Handbook Section 4.7)

32. If lane changes are proposed, identify existing and proposed lane widths and the design speed for each street frontage;

STREET	FROM	TO	LANE WIDTHS Existing / Proposed	DESIGN SPEED
N/A	---	---	--- / ---	---
---	---	---	--- / ---	---
---	---	---	--- / ---	---
---	---	---	--- / ---	---

33. What is the maximum AASHTO design vehicle being accommodated by the design?	SU-40' (Single Unit Truck)	YES <input type="checkbox"/> NO <input type="checkbox"/>	DEPARTMENTAL APPROVAL YES <input type="checkbox"/> NO <input type="checkbox"/>
34. Will the project affect a historically certified street? An <a href="#">inventory of historic streets</a> <sup>(1)</sup> is maintained by the Philadelphia Historical Commission.		YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
35. Will the public right-of-way be used for loading and unloading activities?		YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
36. Does the design maintain emergency vehicle access?		YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
37. Where new streets are being developed, does the design connect and extend the street grid?		YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
38. Does the design support multiple alternative routes to and from destinations as well as within the site?		YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
39. Overall, does the design balance vehicle mobility with the mobility and access of all other roadway users?		YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>

**APPLICANT: Vehicle / Cartway Component**  
Additional Explanation / Comments: \_\_\_\_\_

**DEPARTMENTAL REVIEW: Vehicle / Cartway Component**  
Reviewer Comments: \_\_\_\_\_

(1) [http://www.philadelphiastreet.com/images/uploads/documents/Historical\\_Street\\_Paving.pdf](http://www.philadelphiastreet.com/images/uploads/documents/Historical_Street_Paving.pdf)

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### URBAN DESIGN COMPONENT (Handbook Section 4.8)

40. Does the design incorporate windows, storefronts, and other active uses facing the street?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/>	DEPARTMENTAL APPROVAL YES <input type="checkbox"/> NO <input type="checkbox"/>
41. Does the design provide driveway access that safely manages pedestrian / bicycle conflicts with vehicles (see Section 4.8.1)?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
42. Does the design provide direct, safe, and accessible connections between transit stops/stations and building access points and destinations within the site?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>

**APPLICANT: Urban Design Component**  
Additional Explanation / Comments: \_\_\_\_\_

**DEPARTMENTAL REVIEW: Urban Design Component**  
Reviewer Comments: \_\_\_\_\_

# Complete Street Handbook Checklist

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### INTERSECTIONS & CROSSINGS COMPONENT (Handbook Section 4.9)

43. If signal cycle changes are proposed, please identify Existing and Proposed Signal Cycle lengths; if not, go to question No. 48.

SIGNAL LOCATION	EXISTING CYCLE LENGTH	PROPOSED CYCLE LENGTH
N/A	_____	_____
_____	_____	_____
_____	_____	_____
_____	_____	_____

	YES	NO	N/A	DEPARTMENTAL APPROVAL
44. Does the design minimize the signal cycle length to reduce pedestrian wait time?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
45. Does the design provide adequate clearance time for pedestrians to cross streets?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
46. Does the design minimize pedestrian crossing distances by narrowing streets or travel lanes, extending curbs, reducing curb radii, or using medians or refuge islands to break up long crossings? <i>If yes, City Plan Action may be required.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
47. Identify "High Priority" intersection and crossing design treatments (see Handbook Table 1) that will be incorporated into the design, where width permits. Are the following "High Priority" design treatments identified and dimensioned on the plan?				YES <input type="checkbox"/> NO <input type="checkbox"/>
▪ Marked Crosswalks	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
▪ Pedestrian Refuge Islands	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
▪ Signal Timing and Operation	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
▪ Bike Boxes	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
48. Does the design reduce vehicle speeds and increase visibility for all modes at intersections?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
49. Overall, do intersection designs limit conflicts between all modes and promote pedestrian and bicycle safety?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>

**APPLICANT: Intersections & Crossings Component**  
Additional Explanation / Comments: \_\_\_\_\_

**DEPARTMENTAL REVIEW: Intersections & Crossings Component**  
Reviewer Comments: \_\_\_\_\_

## COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



### ADDITIONAL COMMENTS

**APPLICANT**  
Additional Explanation / Comments: \_\_\_\_\_

**DEPARTMENTAL REVIEW**  
Additional Reviewer Comments: \_\_\_\_\_



# Contact

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